


FROM	NAME & TITLE	THOMAS J. STOSUR, DIRECTOR	CITY of BALTIMORE MEMO	
	AGENCY NAME & ADDRESS	DEPARTMENT OF PLANNING 417 E. FAYETTE STREET, 8 th FLOOR		
	SUBJECT	CITY COUNCIL BILL #18-0270/ PLANNED UNIT DEVELOPMENT -DESIGNATION-5601 EASTERN AVENUE		

TO

DATE: August 7, 2018

The Honorable President and
Members of the City Council
City Hall, Room 400
100 N. Holliday Street

At its regular meeting of August 2, 2018, the Planning Commission considered City Council Bill #18-0270 for the purpose of repealing and replacing the 5601 Eastern Avenue Planned Unit Development #157.

In its consideration of this Bill, the Planning Commission reviewed the attached staff report, which recommended amendment and approval of City Council Bill #18-0270 and adopted the following resolution; nine members being present (seven in favor and two recusals).

RESOLVED, That the Planning Commission concurs with the recommendation of its departmental staff, and recommends that City Council Bill #18-0270 be amended and passed by the City Council.

If you have any questions, please contact Mr. Eric Tiso, Division Chief, Land Use and Urban Design Division at 410-396-8358.

TJS/ewt

attachment

- cc: Mr. Pete Hammen, Chief Operating Officer
- Mr. Jim Smith, Chief of Strategic Alliances
- Ms. Karen Stokes, Mayor's Office
- Mr. Colin Tarbert, Mayor's Office
- Mr. Kyron Banks, Mayor's Office
- The Honorable Edward Reisinger, Council Rep. to Planning Commission
- Mr. William H. Cole IV, BDC
- Mr. Derek Baumgardner, BMZA
- Mr. Geoffrey Veale, Zoning Administration
- Ms. Sharon Daboin, DHCD
- Ms. Elena DiPietro, Law Dept.
- Mr. Francis Burnszynski, PABC
- Ms. Eboni Wimbush, DOT
- Ms. Natawna Austin, Council Services
- Mr. Ervin Bishop, Council Services
- Mr. Alfred Barry, AB Associates



Catherine E. Pugh
Mayor

PLANNING COMMISSION

Sean D. Davis, Chairman

STAFF REPORT



Thomas J. Stosur
Director

August 2, 2018

REQUEST: City Council Bill #18-0270/ Planned Unit Development – Designation– 5601 Eastern Avenue

For the purpose of repealing the existing Development Plan for the 5601 Eastern Avenue Planned Unit Development and approving a new Development Plan for the 5601 Eastern Avenue Planned Unit Development.

RECOMMENDATION: Approval with the following amendments:

- Amend page 1, Recitals: In lines 15 through 19 **strike** the words “that is to be added to ... and final plan approvals from the Planning Commission, and to revise certain text provisions.”
- Amend page 2, Section 3 to **replace** all exhibits C-1.0 through C-7.0 dated June 20, 2018 with all new exhibits C-1.0 through C-7.0, dated July 12, 2018.
- Amend page 2, Section 3 to **add** new exhibit “Exhibit C-8.0, ‘Design Guidelines,’” dated July 12, 2018 after line 22.
- Amend page 2, Section 6 (a), to **read** “Uses allowed in the Planned Unit Development are those uses permitted under Title 10-205 and Table 10-301. In addition, the two following conditional uses are allowed: Retail Goods Establishment (With Alcoholic Beverages Sales) and Gas Station.”
- Amend page 2, Section 6(b) to **read** “1 Retail Goods Establishment (With Alcoholic Beverages Sales) when within a full service grocery store whose net leasable area exceeds 15,000 square feet; and”
- Amend page 2, Section 6, to **strike** “(c) 1 gas station as approve under the Final Development Plan at 5801 Eastern Avenue.”
- Amend page 3, to **strike** Section 8 in its entirety and renumber the sections.
- Amend page 4, Section 11 to **strike** lines 4 through 8, “(b) A maximum of three monument...facing I-95 will be permitted.”
- Amend page 4, Section 13, to **add** language on line 18 after “Ordinance.” the following language: “All Final Design Approvals previously accepted in the existing PUD established in 2015 are accepted under this PUD, subject to other Agency comments.”

STAFF: Tamara Woods

OWNER/PETITIONER: TRP-MCB 5601 Eastern LLC

SITE/GENERAL AREA

General Area:

The existing 5601 Eastern Avenue Planned Unit Development (PUD) is located in the Pulaski Industrial Area, which is adjacent to the Greektown and Bayview neighborhoods of southeast Baltimore. The site is situated on the south side of Eastern Avenue, across from the Johns Hopkins Bayview Hospital and is bounded by Bonsal Street to the east, Umbra Street to the west and I-95 bounds the site to the south.

Site Conditions:

The subject parcel, 5601 Eastern Avenue, is approximately 20 acres (871,751 square feet). The site was improved with several warehouse buildings that have recently been demolished. Within the 20 acres there are significant grade changes, most notably from an upper section fronting Bonsal Street that is roughly 19 feet higher in elevation than that fronting Eastern Avenue. There is also a significant slope from Eastern Avenue to the rear of the site. In addition to the physical characteristics of the site, there are also environmental challenges.

For many years, the Pemco Corporation, as well as other corporations, held this site to manufacture glass and porcelain. Throughout the time it was used for heavy manufacturing waste porcelain and glass, known as “frit,” was disposed of in the western portion of the site. The owner is currently working with the Maryland Department of the Environment to do proper mitigation.

HISTORY

- Ordinance 15-379 was signed by the Mayor on June 24, 2015 to rezone 5601 Eastern Avenue from the M-3 zoning district to the B-2-3 zoning district.
- Ordinance 15-380 was signed by the Mayor on June 24, 2015 to establish the 5601 Eastern Avenue Planned Unit Development.
- On May 4, 2017, the Planning Commission approved the Final Design for the LA Fitness.
- On December 21, 2017, the Planning Commission approved the Final Design proposed retail and site for the Eastern side of the site, as well as a Phase I Signage Package.
- On May 31, 2018, the Planning Commission approved a major subdivision for 5601 Eastern Avenue.

ANALYSIS

Project Overview: The 5601 Eastern Avenue Planned Unit Development (PUD) is a mixed-use commercial and residential development. The development plan proposes the creation of a new mixed use development that includes both a low density and high density option that would be built in phases. As stated before, this site is a former industrial site. The proposed land uses and density in the PUD are based on a 2015 rezoning that rezoned the site from M-3 to B-2-3. At the time of the 2015 PUD approval, the site was one parcel. In May 2018, the Planning Commission approved a major subdivision, which is in the process of being recorded. In addition, the developer is currently under contract to acquire 5801 Eastern Avenue. Their desire is to add this parcel to the PUD and eventually consolidate to the easternmost parcel to create a new retail opportunity that fronts Eastern Avenue.

In 2016, the City Council of Baltimore approved a new zoning code which went into effect on June 5, 2017. In that zoning code new rules for PUDs went into effect and the property of 5601 Eastern Avenue was rezoned to I-2, an industrial zoning. Also, the new zoning code, which went into effect in June 2017, created new rules for PUDs that require repeal and replacement of the PUD for any major change. This requirement to repeal and replace the PUD, necessitates the rezoning of the property because PUDs cannot be established in the I-2 zoning category. Without rezoning, no major changes could be done with the existing PUD. That being said, many of the changes would have required a major amendment to the PUD under the old rules. The transition rules are put in place to require projects to become current to existing zoning rules if the desire is to make substantial changes to the PUD.

City Council Bill #18-0270 / Planned Unit Development – Designation – 5601 Eastern Avenue

As previously noted, Ordinance 15-380 was signed by the Mayor in June 2015 to establish the 5601 Eastern Avenue Planned Unit Development. Though the developer is currently in permits for one building within the PUD and plans to submit permits prior to a new PUD being in place, there are major changes proposed within the PUD legislation from the existing PUD. These changes are:

- Expansion of PUD boundaries to include a new parcel 5801 Eastern Avenue.
- The formal removal of a high-density development plan option (though the development plan was modified via Minor Change in May 2017 to pursue buildout of the low-density development plan option.)
- Modification in the development plan concept for the PUD, including modifying the planned hotel site, for a possible medical office building, inclusion of a new retail/gas station site at 5801 Eastern Avenue (to be combined with previously shown parking lot site.)
- Significant changes to the use structure within the text.

Development Plan: This proposed repeal and replace, like the existing PUD, establishes a comprehensive, yet flexible development plan for the site by establishing a physical layout of proposed improvements on both 5601 Eastern Avenue and 5801 Eastern Avenue, but the text sets maximum square footage and height limits across the development plan.

The proposed limits mirror limits that are in the existing PUD. The total development of the entirety of the PUD area is limited to 2 million SF. In addition to this cap, there is a square footage cap by the type of land uses provided within the PUD. The PUD anticipates a mix of retail, office, residential and hotel. The table below shows the land use cap. The limits are as follows:

Land Use Limits
Retail – 184,000 SF
Office – 376,000 SF
Residential – 1,350 units
Hotel – 350,000 SF

While these limits cap the amount of density allowed per land use within the PUD, the building envelope for any future building is also capped by height limits on the Development Plan. The height zones help to define the street wall and align to the topography of the site which slopes downward toward the rear of the site. The table below shows the height zones, limits and approximate locations.

Zone	Height Limit	Location
No Build Zone	65 foot buffer	Western most edge of site near Greektown
Height Zone 1	45 feet	Low height adjacent to no build zone
Height Zone 2	88.6 feet	Northern band along Eastern Avenue
Height Zone 3	100 feet	Center band of the site
Height Zone 4	140 feet	Southern most section of the site

Land Use: The proposed uses for this PUD in Section 6 of the legislation are contingent upon rezoning the site from the I-2 zoning district to the C-3 zoning district (refer to companion legislation City Council Bill #18-0269). The proposed PUD lists permitted uses and prohibited uses individually. As the bill is written, all uses permitted in the C-3 zoning district would be allowed within the PUD.

Also added is the allowance of a limit of one retail goods establishment when within a grocery store larger than 15,000 SF. Overall, the land use provisions within the bill are in keeping with the character of the surrounding neighborhoods and promote a healthy mix of commercial and residential uses.

Development Plan Reviews: Both Design Review and Site Plan Review are currently underway for this PUD. The PUD master plan, as well as various aspects of the development plan are continuously evolving as there is a current PUD in place which has been used for a current subdivision that has not been recorded and existing permit applications which are under review.

Conformance with Title 13: As part of the analysis of City Council Bill #18-0270, staff reviewed § 13-203 *Approval standards* and §13-204 *Exceptions from district regulations*. Title 13 also states that § 5-405 *Conditions* and § 5-406 *Approval standards* from the Conditional Use criteria must be reviewed in conjunction with the additional requirements of Title 13. This proposal must satisfy the requirements of the governing standards for PUDs per Title 13 and consequently Title 5 of the Zoning Code. Staff offers the following considerations from our review:

- The proposed PUD would not be detrimental to the public health, safety or welfare.
- The proposed PUD is not contrary to the public interest.

- The proposed PUD is in harmony with this code, provided the rezoning to C-3 per the companion bill City Council Bill #18-0269 is passed and goes into effect.
- The proposed mixed-use development meets several of the city’s Comprehensive Master Plan goals, including returning vacant properties to productive use; creating/strengthening main streets; and increasing housing choice.
- The proposed development reflects the mixed-use character of the surrounding neighborhood and incorporates design guidelines that respect and compliment the surrounding neighborhood. Therefore, this proposed PUD should not be detrimental to the character and nature of existing and contemplated development in the immediate area.
- The location of the PUD has been thoughtfully considered to accommodate the existing topography of the land. There are tremendous slopes across the entirety of the PUD. The established heights and proposed development plan take the topography into consideration.
- The proposal will not create any situation that should negatively impact future development potential or the use, maintenance or value of neighboring areas already developed.
- The new construction will not impact the availability of light, air, open space, and street access.
- The proposed development will not reduce the protection of residents, visitors, or neighboring residents from fire, health hazards, or other dangers.
- The proposed PUD and development plan provide substantial benefits to the City, as it will bring a vacant highly contaminated manufacturing site back to viability by provided a mixture of new uses that include new infrastructure within the site, improved streetscape, intersection and signal improvements along Eastern Avenue, services, gathering spaces, as well as, portions of naturalized environment for citizens to use and enjoy.

Legislative Amendments: There are several amendments that Planning Staff is proposing to this bill. The amendments and purpose of them are as follows:

1. Amend page 1, Recitals: In lines 15 through 19 **strike** the words “that is to be added to ... and final plan approvals from the Planning Commission, and to revise certain text provisions.”
2. Amend page 2, Section 3 to **replace** all exhibits C-1.0 through C-7.0 dated June 20, 2018 with all new exhibits C-1.0 through C-7.0, dated July 12, 2018.
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5. Amend page 2, Section 6(b) to read "1 Retail Goods Establishment (With Alcoholic Beverages Sales) when within a full service grocery store whose net leasable area exceeds 15,000 square feet; and"
6. Amend page 2, Section 6, to strike "(c) 1 gas station as approve under the Final Development Plan at 5801 Eastern Avenue."
7. Amend page 3, to strike Section 8 in its entirety and renumber the sections.
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9. Amend page 4, Section 13, to add language on line 18 after "Ordinance." the following language: "All Final Design Approvals previously accepted in the existing PUD established in 2015 are accepted under this PUD, subject to other Agency comments."

Purpose of Amendments 1 and 6

Proposed Amendments 1 and 4 are regarding the Final Design Approval of a property, 5801 Eastern Avenue, that is not currently in the existing PUD that will be adjoined in the future to a parcel that is currently in the PUD. The establishing PUD for this property cannot at its creation also approve the Final Design that per language of this bill and the existing PUD are in the authority of the Planning Commission. Any development plans for new construction included in the PUD exhibits is for concept only. The project will require Final Design Approval by the Planning Commission in the future.

The intended developer of the retail space is not the petitioner of this PUD, though the petitioner of the PUD is purchasing 5801 Eastern Avenue. Planning Staff has had a pre-development meeting with the intended developer to thoroughly review the project or ascertain exactly which approvals would be needed. In addition, the engineer for the site, MRA, attended one Site Plan Review Committee meeting on July 11, 2018, one day prior to the July 12, 2018 Planning Commission hearing. Design Review has just begun.

Purposed of Amendments 2 and 3

The exhibits as introduced have been updated to reflect changes made since introduction. These include changes to the development plan to show that the plan for the eastern-most edge is conceptual, updates to show the current zoning code classification and proposed boundaries to the Forest Delineation Plan and an updated landscape plan exhibit. All of the exhibit sheets should be replaced with new dates so as not to confuse sheets upon approval of the PUD.

In addition, a new exhibit "C-8.0 Design Guidelines" is to be added (See Appendix). This document is included in the existing PUD and was inadvertently not included in the introduction of the current PUD City Council Bill #18-0270. The design guidelines include language on parking, building form and placement and general planting. They also include streetscape design guidance for various zones: Frontage Zone, Pedestrian Zone, Public Amenity Zone and Roadway Edge Zone. The guidelines speak to screening, street furniture and other items within the public spaces. They are meant to be used in conjunction with the PUD text and development plan. Together they illustrate the intent of the design and character of any future structures within the PUD.

Purpose of Amendment 4 and 5

These amendments pertain to the allowable uses within the PUD. The uses align to the presumed rezoning of 5601 Eastern Avenue to C-3. The allowable use list must be amended to allow for two conditional uses under the zoning code. "Retail Goods Establishment (With Alcoholic Beverages Sales) and Gas Station. In addition, amendment five is just to match the use name to what is it called in the zoning code. Amendment 6 is not needed as the

Purpose of Amendment 7

Liquor licenses are governed by the State of Maryland and the Liquor Board. The Board of Municipal and Zoning Appeals does not issue conditional uses for liquor licenses as alluded to in (a) of Section 8. The section is not needed as the PUD will be allowed to receive the proper licenses per the Liquor Board. The limit on the number of establishments allowed is included within the land use section of City Council Bill #18-0270.

Purpose of Amendment 8

This section is not needed. The total amount of signage is calculated for the entirety of the PUD. The development plan show the locations of the proposed monument signs. In addition, the proposed Amendment 6 will allow for any previous approvals to be accepted, which include two of the proposed monument signs.

Purpose of Amendment 9

As previously mentioned, there is an existing PUD for this property that had current Final Design Approvals received in 2017. In addition, the developer has submitted for permits for the LA Fitness, that is the subject of the May 2017 Planning Commission Approval. It is expected that the developer may submit for permits under the existing PUD prior to the passage of CCB #18-0270 or within weeks of passage. This language will allow for continuity of prior Final Design Approvals as they transition to a new PUD. The intent is that the design will stand, but subject to any other agency reviews that may require a change. If the developer changes the designs substantially, new Planning Commission approval would be required.

Notifications:

In advance of a hearing on this matter, staff notified the Community Leaders of Bayview, representatives from the following associations: the Bayview Community Association, Southeast Neighborhoods Development (SEND) Southeast CDC, the Greater Greektown Neighborhood Alliance and City Councilman Zeke Cohen.



Thomas J. Stosur
Director

**Appendix
Amendment #6
Design Guidelines Exhibit C-8.0**

DESIGN GUIDELINES

5601 and 5801 EASTERN AVENUE

BALTIMORE, MD 21224

BUSINESS PLANNED UNIT DEVELOPMENT PLAN

EXHIBIT C-8.0

JULY 12, 2018

Note: The illustrations contained in this document show the design intent, but do not represent a formal approval of any site or building design. The illustrations contained herein are conceptual and guidelines only, and are subject to Final Design Approval. Specific references (names) to internal circulation streets and drives are for descriptive purposes only.

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DESIGN PRINCIPLES

5601 Eastern Avenue Business Planned Unit Development

INTRODUCTION

These design principles are intended to support the overall intent behind the specific guidelines that accompany the Planned Unit Development approval of 5601 Eastern Avenue and its redevelopment into a mixed-use center with retail, office, fuel service station/convenience store, medical, hotel and residential uses.

The 5601 Eastern Avenue Business Planned Unit Development consist of the parcels addressed as 5601 Eastern Avenue and 5801 Eastern Avenue but will be referenced to as 5601 Eastern Avenue.

1. An integrated redevelopment plan

The entire site of 5601 Eastern Avenue will be designed as a cohesive redevelopment plan with integrated design and architectural features that complement each of the individual phases and the surrounding residential and institutional communities. The plan is intended to be a phased project within four distinct Development Areas (Design Guidelines Exhibit A).

2. Create a pedestrian –friendly, mixed-use environment

Provide significant features such as welcoming and enhanced pedestrian areas that facilitate movement both to the new development from the community and within the development itself. This principle also recognizes the importance of the architectural treatments such as windows and entrances within the plan to reinforce and welcome pedestrian activity.

3. Create a high-quality environment

The use of high-quality design elements and materials in both the architectural and landscaping elements throughout the redevelopment plan are intended to promote a quality destination environment that emphasizes the redevelopment as an asset that reinforces the on-going revitalization of the overall community.

4. Undertake the environmental remediation and planning of the entire site as the initial phase

The entire 20.6-acre property will undergo remedial action and planning with Maryland Department of the Environment (MDE) and U.S. Environmental Protection Agency (EPA) approval prior to commencement of any of the redevelopment phases.

5. Complete the initial landscaping areas following environmental remediation

The landscaped areas along both Eastern Avenue and the buffer adjacent to the Greektown community (Design Guidelines Exhibit B) will be implemented as part of the initial phase of redevelopment in order to establish the overall landscaping intent.

STREETSCAPE DESIGN

The objective is to create streetscape designs that focus on improving the attractiveness and effectiveness of the pedestrian network with a level of safety, social spaces, amenities, and active uses that reinforce and encourage pedestrian activity along the street corridor.

- Streetscape at retail/restaurant frontage areas must accommodate pedestrian activities and allow retail/restaurant programs to spread out into the hardscape area.
- The streetscape design must screen parking lots from adjacent public rights-of-way. In order to screen headlights from the parked car a 3.5'+/- height wall or landscape screening buffer will be required facing Eastern Avenue. The parking lot shall be graded to be lower than the sidewalk along Eastern Avenue to enhance the screening. Walls should be constructed out of durable, low maintenance materials coordinated with the materials used on building facades. Walls should be placed on the inside edge of the landscape strip with the planting on the Eastern Avenue side of the wall or in wider buffers, placed to allow planting on both sides of the wall.
- An 8' wide clear unobstructed, linear sidewalk space must be created to allow for pedestrian circulation. If there are project constraints effecting the design, an ADA compliant, unobstructed walking path must maintained. Where space allows within the pedestrian zone street furniture is encouraged as long as the minimum clear zone is maintained.
- The area at roadway edge should be clear of any amenity element and should be set back as much as 3 feet from the face of the street curb to avoid conflict with on-street parking (e.g. car doors, passenger loading, etc.), but no less than 1.5 feet.

General Sidewalk Design

Sidewalk widths are established at 8 feet. If there are project constraints effecting the design, an ADA compliant, unobstructed walking path must maintained. Public sidewalks will provide a direct and continuous pedestrian network with a clear, unobstructed pedestrian path way throughout the development. Sidewalks will be cast-in-place concrete or integral colored concrete with thoughtful score line design.

Paving Design

The pedestrian experience shall be enhanced through the variation of paving materials. Paving material, color, and pattern shall provide visual cues to help define pedestrian access and safety. The surfaces shall be attractive, durable and of high-quality. Design options for paving surfaces include: cast-in-place concrete, integral colored concrete, stamped concrete, non-slip finish concrete, and stamped asphalt.

Colors to be coordinated with architecture.

- Special paving shall be located at high visibility locations like entrances and intersections

Lighting Selection and Design

All lighting shall be dark sky compliant. When possible, street lighting shall be pedestrian

oriented 12'- 15' high decorative poles. To provide additional pedestrian lighting and night light effects, accent lighting should be incorporated when appropriate. Wall washes, LED colored light, light strings, and pendant lights are encouraged at entries and gathering areas. When required for vehicular safety, cobra lighting, or a similar vehicular scale fixture, shall be installed.

General Planting Guidelines and Design

To enhance the pedestrian experience, planting of the public spaces adds visual interest, softens the urban edges, and provides shade. It also creates buffers, pedestrian scale, and assists with storm water management. When selecting a planting palette, consideration shall be given to compatibility with the urban environment (i.e. limited space for roots and canopies, limited soil fertility increased urban pollution, etc.) and multi-seasonal interest.

Streetscape planting shall have a consistent and well-coordinated planting scheme where specific species are repeated regularly throughout the site to provide visual continuity and identity of spaces. The plant palette selected should assist in unifying the site while also offering periodic accent points. Landscaping should not obstruct pedestrian through-traffic or access to the street or retail storefronts. Spacing of trees shall coordinate with utilities, sight clearances, and maintaining views of retail signage and storefronts.

Proper clearance of plant material includes the following:

- Shrubs shall be trimmed to three (3) feet or less in height above the grade of the sidewalk
- Tree canopies shall be trimmed up to at least eight (8) feet over the sidewalk when trees have matured.
- Street trees shall be selected that have a branching pattern that will not obscure storefront windows or conflict with truck/pedestrian access.
- Where possible, trees shall be spaced 25'-35' to maintain a consistent overhead canopy; provided, however, final tree locations shall be coordinated with utilities.
- Tree pits shall be a minimum of 4'x8' and may include continuous planting strips or pits.

STREETSCAPE ZONES

For purposes of these guidelines, the pedestrian realm has been subdivided into four zones: the frontage zone, the pedestrian zone, the amenity zone and the roadway edge zone. These zones are conceptual, and may vary in areas along the street.

- Frontage zone-adjacent to building frontages and serves as a transition area
- Pedestrian zone- primarily accommodates pedestrian circulation and ensures an 8' unobstructed sidewalk width for pedestrian through-traffic (If design constraints are present, at a minimum, the path shall be an ADA complaint walking path.)
- Amenity zone- adjacent to the street and accommodates a landscape buffer, storm water management, and street furnishings (Minimum 4' Width)
- Roadway edge zone- this zone's purpose is to allow cars parked along the street enough room to open their car doors without any obstructions (Minimum 1.5' Width)

Frontage Zone

The frontage zone forms the outer edge of the public right-of-way and typically is defined by a building façade, constructed wall, or landscape screening treatment. This zone provides a connection of pedestrian circulation of the public sidewalk to the interior of the buildings. It allows for retailers and restaurants to extend their programs out into the hardscape. Some uses or programs include displays, street furniture, seating, tables, planters, and other elements as a means of engaging passersby and activating the public streetscape. This area is an opportunity for places of interest like outdoor dining without constricting the adjacent flow of pedestrian traffic.

The streetscape design must screen parking lots from adjacent public rights-of-way. In order to screen headlights from the parked car a 3.5' +/- height wall or landscape screening treatment will be required with landscaping in front of the wall facing Eastern Avenue. Walls shall be constructed of durable, low maintenance materials coordinated with the materials used on building facades or coordinated with the existing church facade. Walls shall be placed on the inside edge of the landscape strip with the planting on Eastern Avenue side of the wall or in wider buffers, placed to allow planting on both sides of the walls.

Pedestrian Zone

The pedestrian zone's intent is to facilitate the efficient movement of pedestrians between the frontage zone and the public amenity zone. It provides a wide unobstructed, linear sidewalk space that is free of street furniture, street trees, planters, and other vertical elements (Refer to General Sidewalk Design listed above). Where space allows within the pedestrian zone street furniture can be introduced as long as the minimum, ADA compliant, clear zone is maintained. The pedestrian zone should generally remain straight, and never be less than ADA compliant at specific constrained locations.

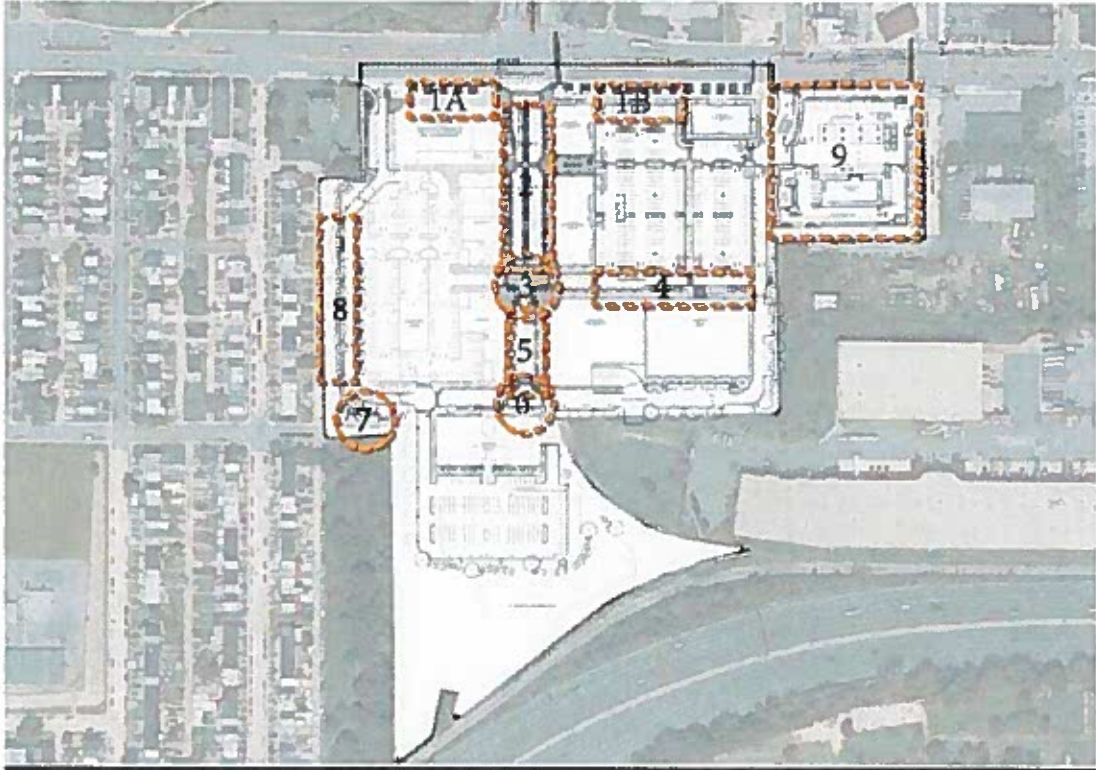
Public Amenity Zone

The public amenity zone contains streetscape amenities that work and coordinate with the pedestrian zone. It includes features such as street trees, landscaping, micro-bioretenion, site furniture, seating, and other features. This space not only acts as a buffer from vehicular traffic but enhances the aesthetics of the streetscape and activates the space with its designated functions. Streetscape features should be repeated to provide a continuity and identity along the street. Where possible and appropriate, furniture shall be oriented to allow for social interaction. Amenities located in this zone improve the visual appearance of the street, and assist in making the streetscape a functional/ active space. Street furnishing design, type, color and material shall be consistent. Along Eastern Avenue there shall be a sufficient landscape buffer provided which may include storm water management areas.

Roadway Edge Zone

The public amenity and the roadway edge zone is the area directly adjacent to the street and buffers pedestrians from the street. Where feasible in the roadway edge zone, elements within the Public Amenity Zone generally should be set back at minimum 1.5' to 3' feet from the face of the street curb to avoid conflict with on-street parking (e.g. car doors, passenger loading, etc.),

AREA LOCATION MAP

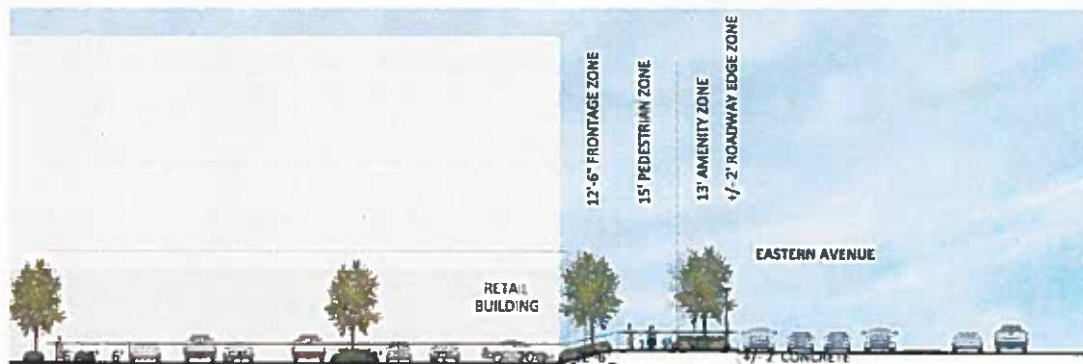
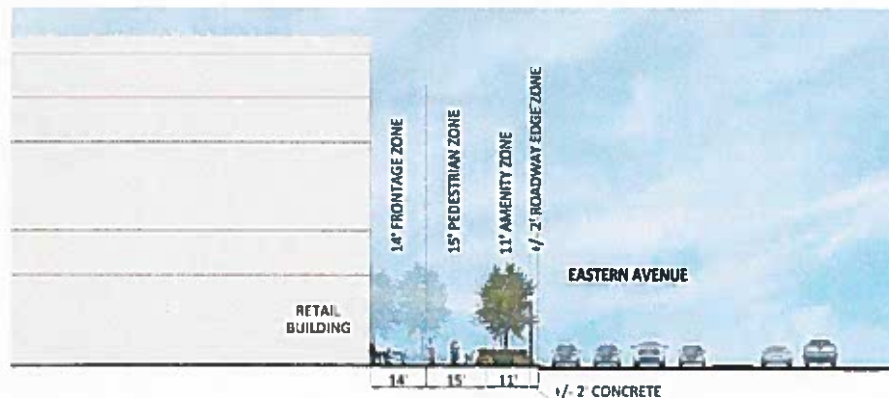


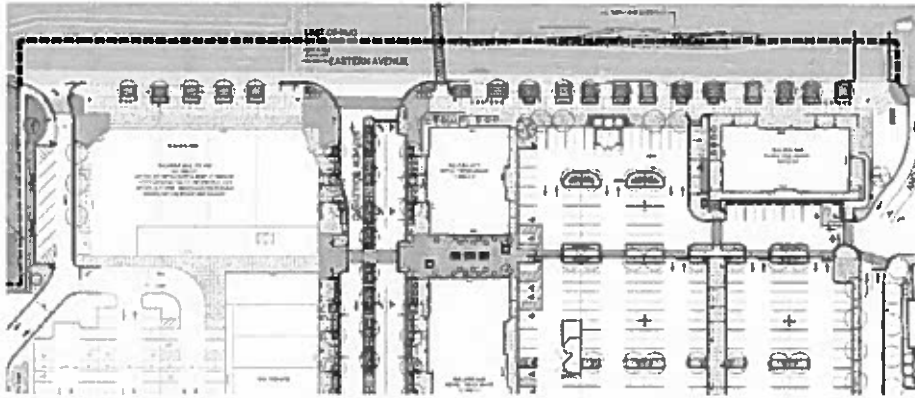
1. STREETScape ON EASTERN AVENUE

The streetscape along Eastern Avenue (shown on Design Guidelines Exhibits B and C) shall maintain an average 42' +/- setback from the travel lanes. This setback was created to coordinate with the adjacent church to the west of the property. Where appropriate, existing pedestrian access and crosswalks shall be incorporated into the pedestrian access areas and create a link to the community.

- Encourage locations that coordinate with public transportation spots.
- Maintain an average 42' +/- setback from travel lanes.
- Lighting along Eastern Avenue shall be coordinated with the project design.
- Provide bike racks and benches.
- Create a landscape buffer along Eastern Avenue; landscaped area can be designed as a micro-bioretention area.

No permanent or temporary surface parking lot shall be permitted within 42 feet of the Eastern Avenue travel lanes.

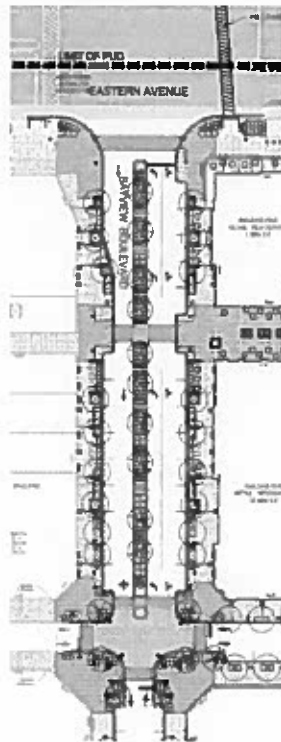


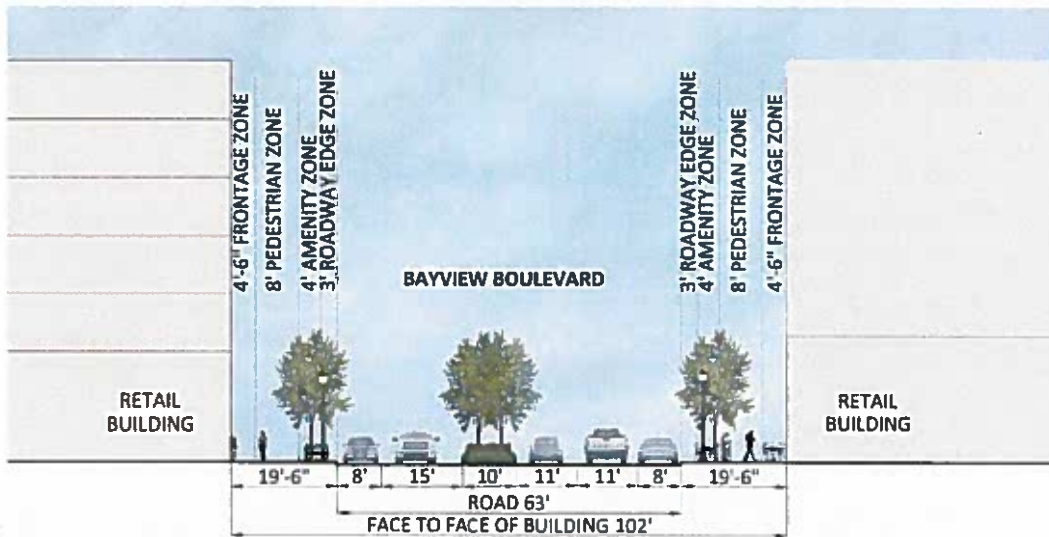


*All dimensions are approximate and subject to change.

2. UPPER BAYVIEW BOULEVARD DESIGN

The focus of the design is on the aesthetics and effectiveness of the pedestrian network that encourages pedestrian activity along the street corridor. Pedestrian elements such as site furnishings and wide sidewalks shall be incorporated to accommodate social interaction and amenities like outdoor dining. Provide benches and smaller seating areas throughout the streetscape. Street trees along this street shall be larger in size ranging from 3"-3.5" caliper to provide a larger tree canopy. Upper Bayview Boulevard Streetscape (Design Guidelines Exhibit C) will also include a 10' +/- median that provides a refuge for crossing pedestrians between retail spaces.

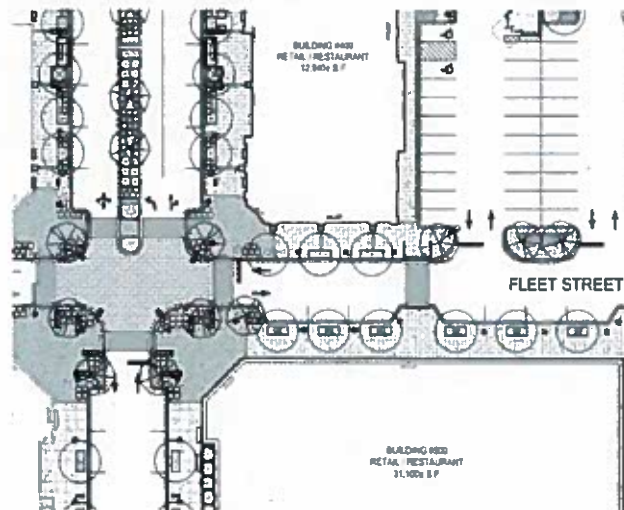


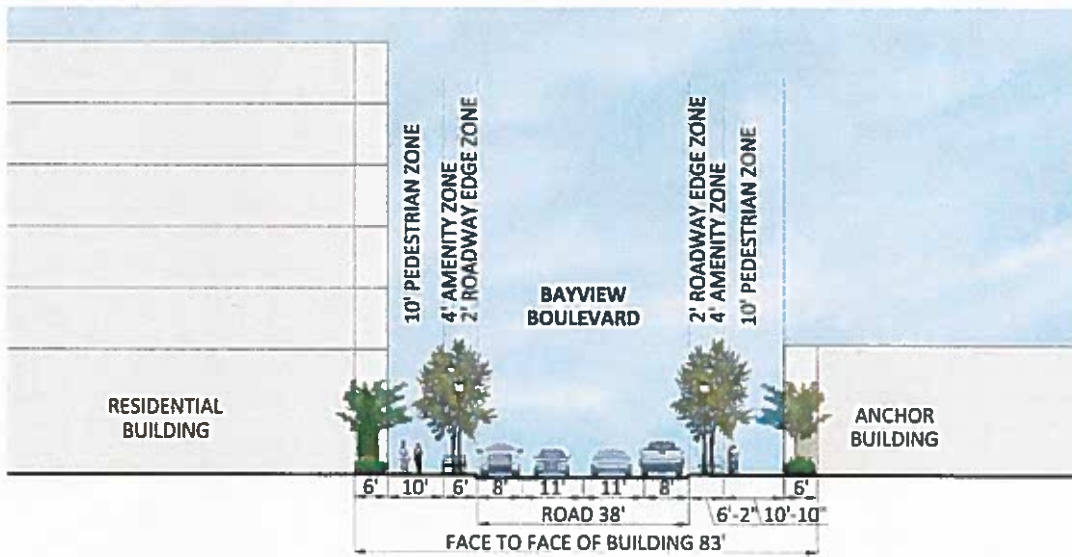
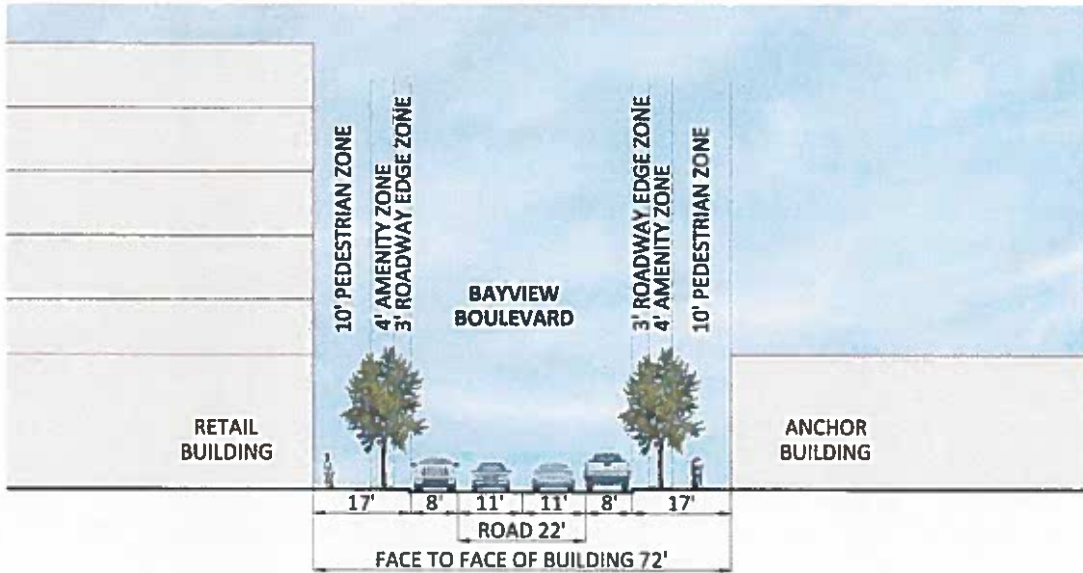


*All dimensions are approximate and subject to change.

3. BAYVIEW BOULEVARD AND FLEET STREET INTERSECTION DESIGN

This intersection (Design Guidelines Exhibit C) is the terminus point of the designated retail area and therefore needs to have a special identity. The buildings in this area will be chamfered to accommodate opportunities for pedestrians to activate the area. This will also allow retailers and restaurants to extend their programs out into the hardscape. This may include displays, street furniture, seating, tables for outdoor dining areas, planters, and other elements as a means of engaging passersby. This intersection shall have special paving patterns and attractive pedestrian crosswalks. To help aid in traffic calming and add a distinctive character in this area, raised planters or seat walls will help separate the pedestrian and vehicular traffic. Site furniture, lighting, seating areas, and outdoor dining is encouraged in this area. The area shall provide a Tree canopy and opportunities for accent planting.

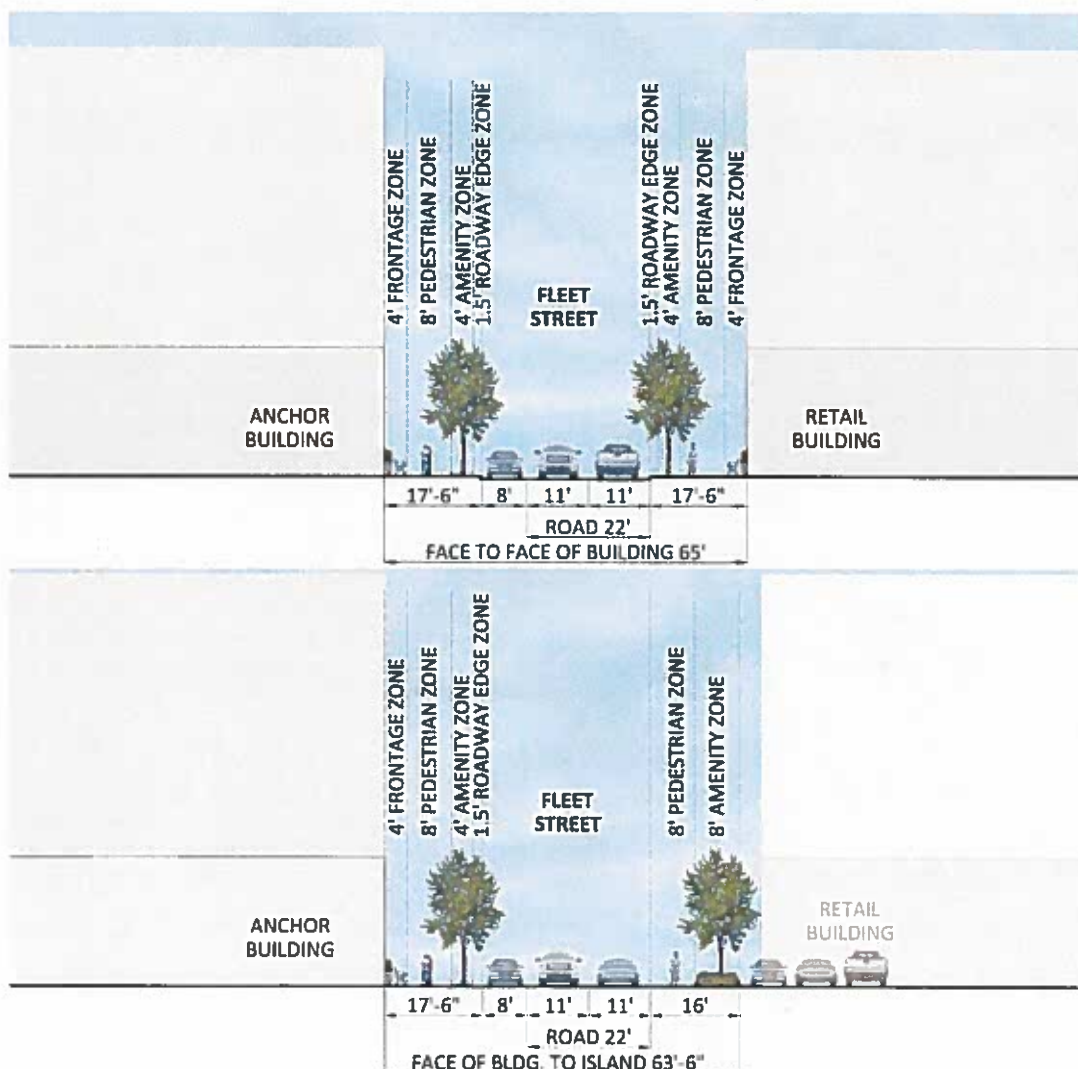




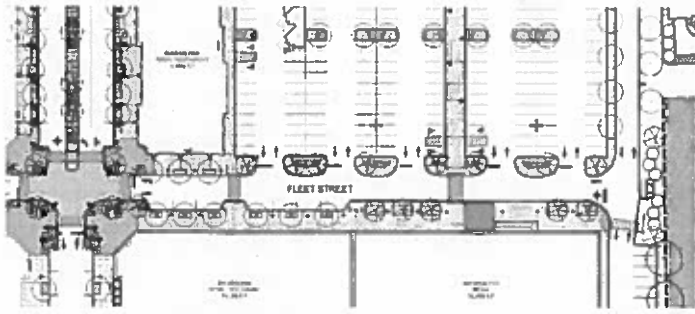
*All dimensions are approximate and subject to change.

4. FLEET STREET ADJACENT TO ANCHOR BUILDING STREETSCAPE DESIGN

This area (Design Guidelines Exhibit C) is designated to act as: 1) a buffer between the parking lot and the anchor building entrance; or, 2) a drive aisle serving surface and structured parking. The use of special paving and bollards is encouraged to help identify the space as a pedestrian friendly walkway. Bollards can be used to mark pathway entries and the bollard styles and color shall match other site furniture. At the far east of Fleet Street, a focal point shall be created to be visible from the Bayview Boulevard and Fleet Street intersection. This focal point can be created by a special landscaped area or a feature wall which may have special signage and lighting.

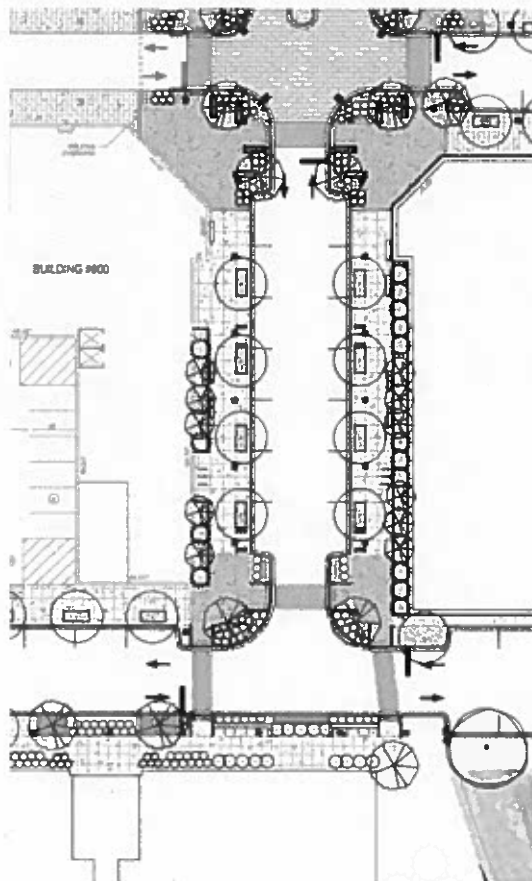


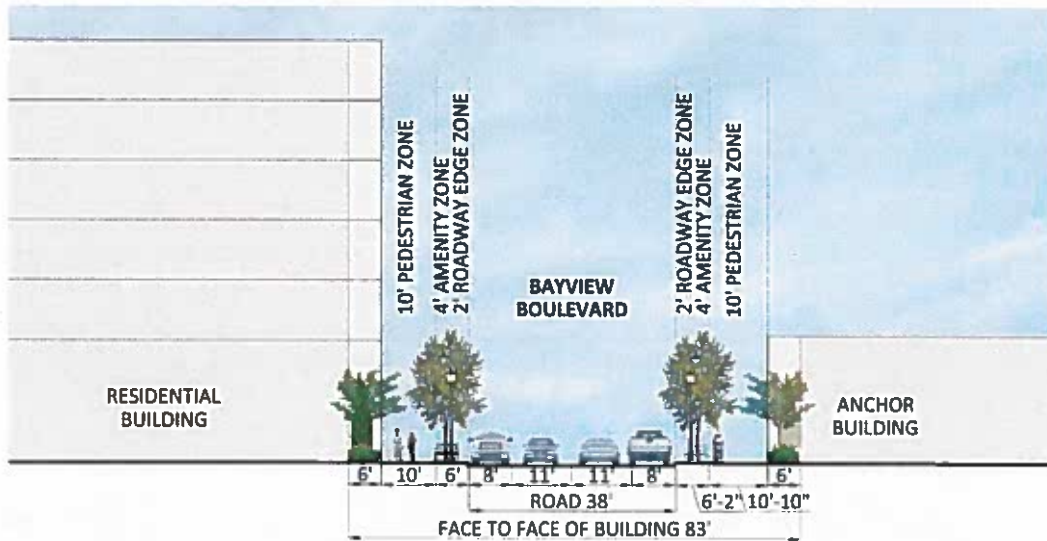
*All dimensions are approximate and subject to change.



5. LOWER BAYVIEW BOULEVARD STREETSCAPE DESIGN

The character transitions here (Design Guidelines Exhibit C) from a retail corridor to an outdoor residential amenity space. This “quieter space” is comprised of green space, seating areas and site furniture such as bike racks. The buildings will be inset 6’ +/- to allow room for larger landscaping areas and seating areas. The road also transforms its character by changing the paving material to a different paving material than the retail areas. Here the site furniture will serve a more flexible purpose such as platforms, seat walls or site furniture. This area will act as a flexible outdoor area for lounging, seating and socializing with other residents and patrons of the site.

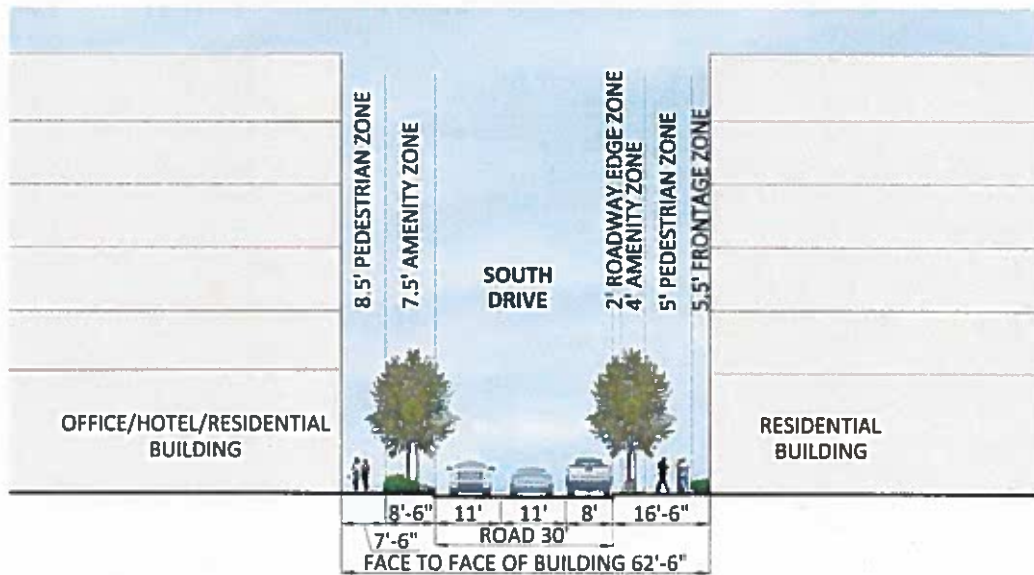
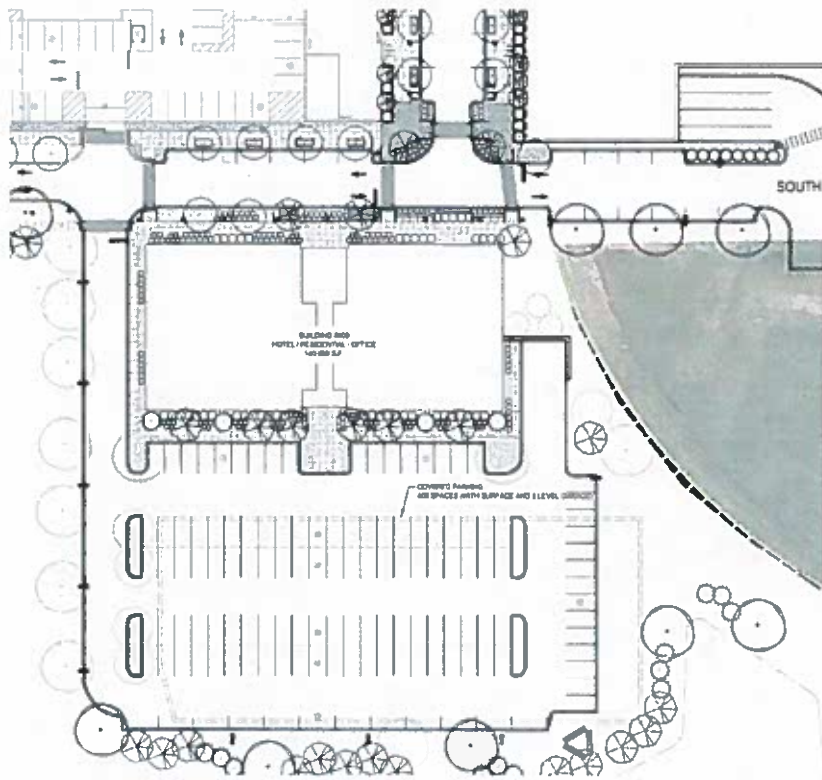




*All dimensions are approximate and subject to change.

6. BAYVIEW BOULEVARD STREETScape AND TERMINUS POINT DESIGN

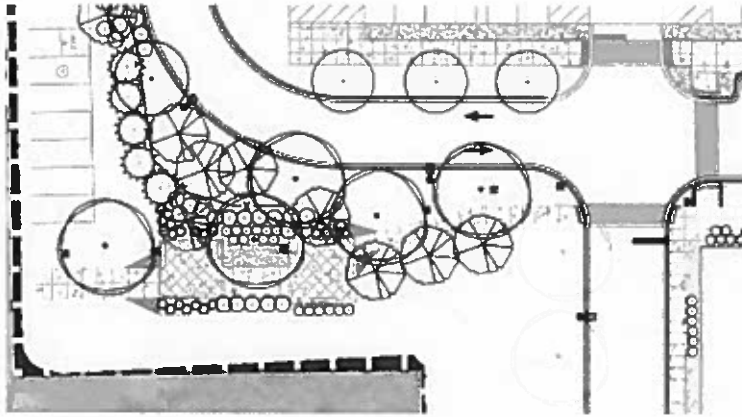
Located at the terminus point of Bayview Boulevard a focal point is to be created at the intersection of Bayview Boulevard and South Drive (Design Guidelines Exhibit C). This will be achieved with a site feature such as a sculpture, architecture, seat wall, water feature, landscaping, etc. Additional pedestrian lighting, night light effects, or accent lighting shall be incorporated into the gathering area. This seating area provides an opportunity for the hotel/residential building to have an amenity of outdoor dining and a place for socializing surrounded by landscaping. To the south of the anchor building, the service road on South Drive shall be screened with landscaping. A wall may be used in this area for additional screening, subject to final design approval. Walls shall be constructed of durable, low maintenance materials coordinated with the materials used on building facades.

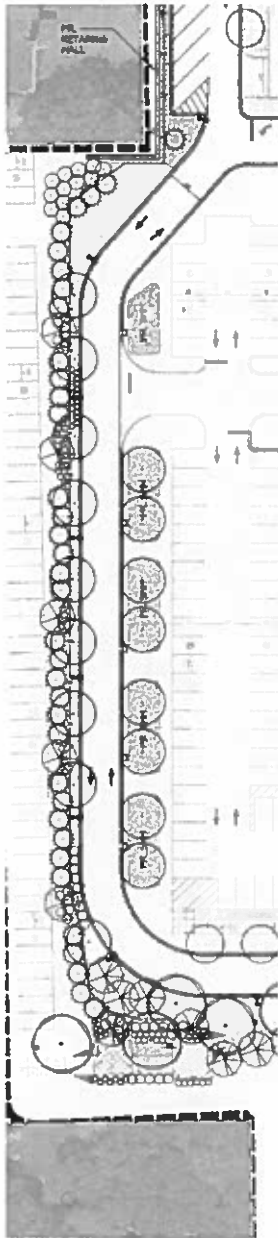


*All dimensions are approximate and subject to change.

7. GREEKTOWN CONNECTION TO SOUTH DRIVE DESIGN

This site is within easy walking distance of residential areas and Greektown. To gain entrance to the site, pedestrian access will be provided that connects Foster Avenue to South Drive. As appropriate, this area shall provide space for passive gathering with benches for seating and social activities. Landscaping shall be low maintenance and must keep clear sightlines so that the seating area can be seen by the hotel/residential building. Shrubs and groundcover must be maintained 3' high or lower so vegetation does not obstruct visibility. Security must be maintained and adequate and appropriate lighting must be provided for pedestrian safety.



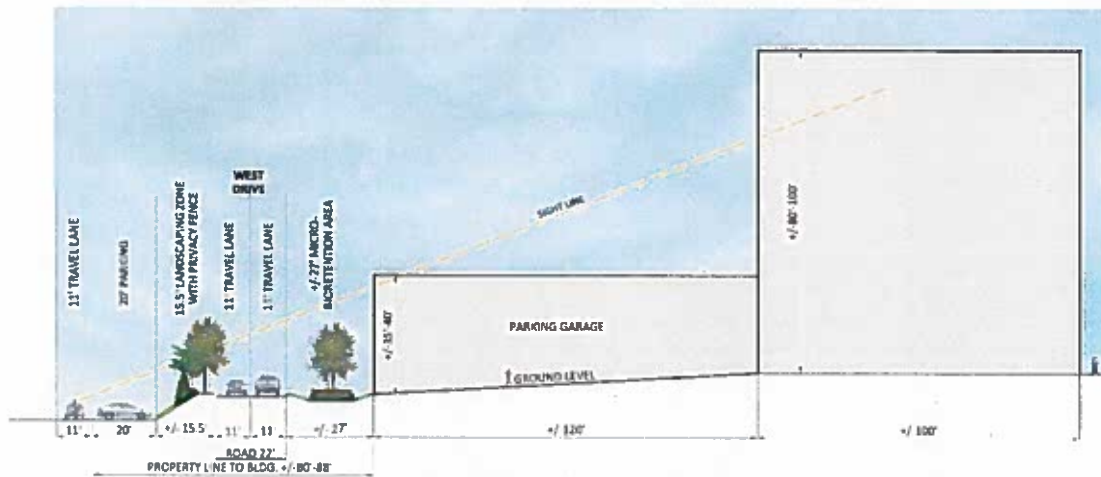


8. GREEKTOWN BUFFER TO WEST DRIVE DESIGN

On the site there is a transitional space located between the residential units of Greektown and the parking garage building on West Drive (Design Guidelines Exhibit B). This area will act as a buffer and transitional zone between the residential community and the myriad of uses on the site. Along the existing alley there will be designated parking for the residents to the west of the property.

Next to the parking an approximately 13'+/- wide landscape buffer shall be created to help screen the new development from the residents. The buffer shall be enhanced by creating the vegetative screen on top of an earthen berm. The sloped area will help screen the residents while assisting with site grading. This vegetative buffer shall include a combination of evergreen trees, shrubs, and shade trees. The majority of the selected plants shall be evergreen to maintain screening qualities in winter. Beyond the landscape buffer a 6' high privacy fence shall be placed (and shown on a site plan prior to final approvals). The privacy fence shall be made of durable, good quality, and aesthetically pleasing materials. It should be coordinated in style, color, and materials of the buildings' architecture.

Stormwater management areas such as micro-bioretenion areas will be provided along both sides of West Drive. The width of the micro-bioretenion areas vary from 20'-27'. As noted above in the "General Planting Guidelines and Design" species selected should be native/ adaptive plant material and should take into account seasonal interest and survivability of the urban condition. Plant species selected for the micro-bioretenion should handle the specific fluxuating water depth.



*All dimensions are approximate and subject to change.

9. EASTERN AVENUE FUEL STATION AND CONVENIENCE STORE

At the corner of Eastern Avenue and Bonsal Street there will be a fuel service station and convenience store placed at the front of the site. The streetscape planting along Eastern Ave will be continued and established as per the streetscape design described in Section 1 (page 8). The streetscape design must screen parking lots from adjacent public rights-of-way. In order to screen headlights from the parked cars a 3.5' +/- height wall and landscape screening treatment will be required with landscaping in front of the wall facing Eastern Avenue. Walls shall be constructed of durable, low maintenance materials coordinated with the materials used on building facades or coordinated with the existing church facade. Walls shall be placed on the inside edge of the landscape strip with the planting on eastern Avenue side of the wall or in wider buffers, placed to allow planting on both sides of the wall. This area will incorporate aesthetically pleasing plantings selected to fluctuate due to seasonal patterns while still providing year-round screening and buffering from the adjacent road. Within the fuel service station/convenience store there will be decorative crosswalks for aesthetics and for pedestrian safety. Crosswalks can also be found surrounding the area for increasing the walkability of the site and providing connectivity to the surrounding neighborhood.

NEW BUILDING DESIGN GUIDELINES

BUILDING FORM AND PLACEMENT

New buildings in the project are to be designed to allow for a transition from nearby low-density areas to moderate and high densities within the site boundary. Building form and penetrations shall be used to define and reinforce the public realm. Build-to lines are established to reinforce the building-to-building relationships (Design Guidelines Exhibit D). Maximum heights and structured parking shall be setback from the existing residential community on the western

boundary of the project.

Design Guidelines:

- New buildings along Upper Bayview Boulevard are to be set back a minimum of 17-19 feet from the curb to the building.
- New buildings along Lower Bayview Boulevard are to be set back a minimum of 15-17 feet from the curb to the building with an additional 6' +/- inset to allow for additional, quiet public realm space.
- Corner buildings at Bayview Boulevard and Fleet Street shall have massing and active fronts reflecting their corner position.
- Buildings located on Development Area C shall have pedestrian access directly to the Bayview Boulevard terminus area as shown on Exhibit C.
- Pedestrian passageways from parking areas to streets are to cut through the building mass and receive appropriate lighting and architectural design consideration.
- Service and utility access shall not be located on Eastern Avenue and Bayview Boulevard.
- All buildings fronting Eastern Avenue and Bayview Boulevard shall have main entrances on those streets.

BUILDING WALL

- 80% of the ground level building wall planes highlighted on Design Guidelines Exhibit D shall meet the build-to line dimension on Exhibit D. The line relationship may be established by building-to-building dimension or by half of the building-to-building dimension from the street center line.
- Street frontage buildings on Eastern Avenue and Upper Bayview Boulevard shall, in their ground level street front facade, allow for potential retail, restaurants, cafes and service type businesses that activate the street with pedestrian traffic. Extensive solid wall areas at the street level are not permitted.
- Street frontage at the intersection of Bayview Boulevard and Fleet Street shall be active, composed of significant amounts of glazing and have entries at the intersection areas.
- Glass at the ground floor must be clear vision glass; heavily tinted or mirrored glass is not permitted.
- Upper floors must have street-facing windows.
- A distinct base will be composed of the first floor or the first two floors of the building.
- Rooftop elements such as mechanical penthouses shall be compatible and integrate into the massing and exterior wall design below in materials, color and design.
- Ground floor facades shall be designed to accommodate signage that allows for each commercial tenant to achieve a distinct identity.

PARKING

STRUCTURED PARKING

- Garage openings should have some form of screening to block views of cars on upper levels.
- Structured parking shall not exclusively front, nor be the first building wall plane, on Eastern Avenue or North South Street.

SURFACE PARKING

- Structured parking is preferred over surface parking.
- No off-street surface parking shall be permitted within 50 feet of the streetscape edge of Bayview Boulevard.
- Permanent surface parking is permitted provided lots: 1) have landscaped refuge islands; and, 2) are screened with constructed walls.
- Surface parking lot light fixtures shall be dark sky compliant.
- Permanent, clear-to-sky, surface parking lots on Development Area A as shown on Exhibit A shall not exceed 100 spaces. Temporary surface parking shall be allowed.

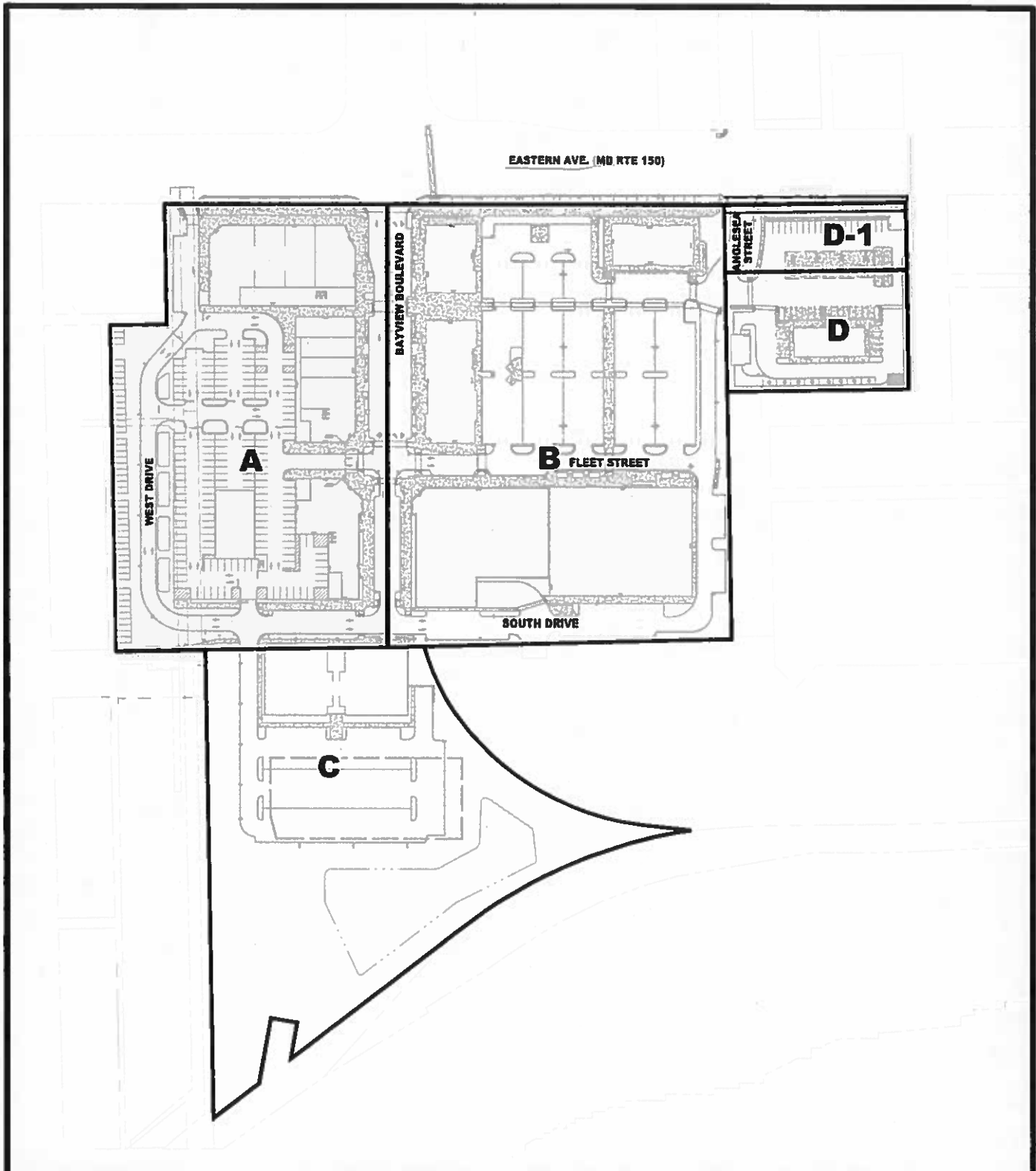
SIGNAGE

The primary project entrance and identity signage locations shall be shown on the Development Plan and have dimensions not to exceed 12' by 4' by 25'.

Comprehensive Signage Plan shall be subject to Final Design Approval.

5601 EASTERN AVENUE, BALTIMORE, MD
21224 BUSINESS PLANNED UNIT DEVELOPMENT

EXHIBITS



MRA

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 1220-C EAST JOPPA ROAD, SUITE 505
 TOWSON, MARYLAND 21286
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 FAX: (410) 821-1748

MRAGTA.COM

5601 EASTERN AVENUE
DESIGN GUIDELINES
EXHIBIT 'A'
 DEVELOPMENT AREAS

BALTIMORE CITY, MARYLAND

SCALE: 1"=200'

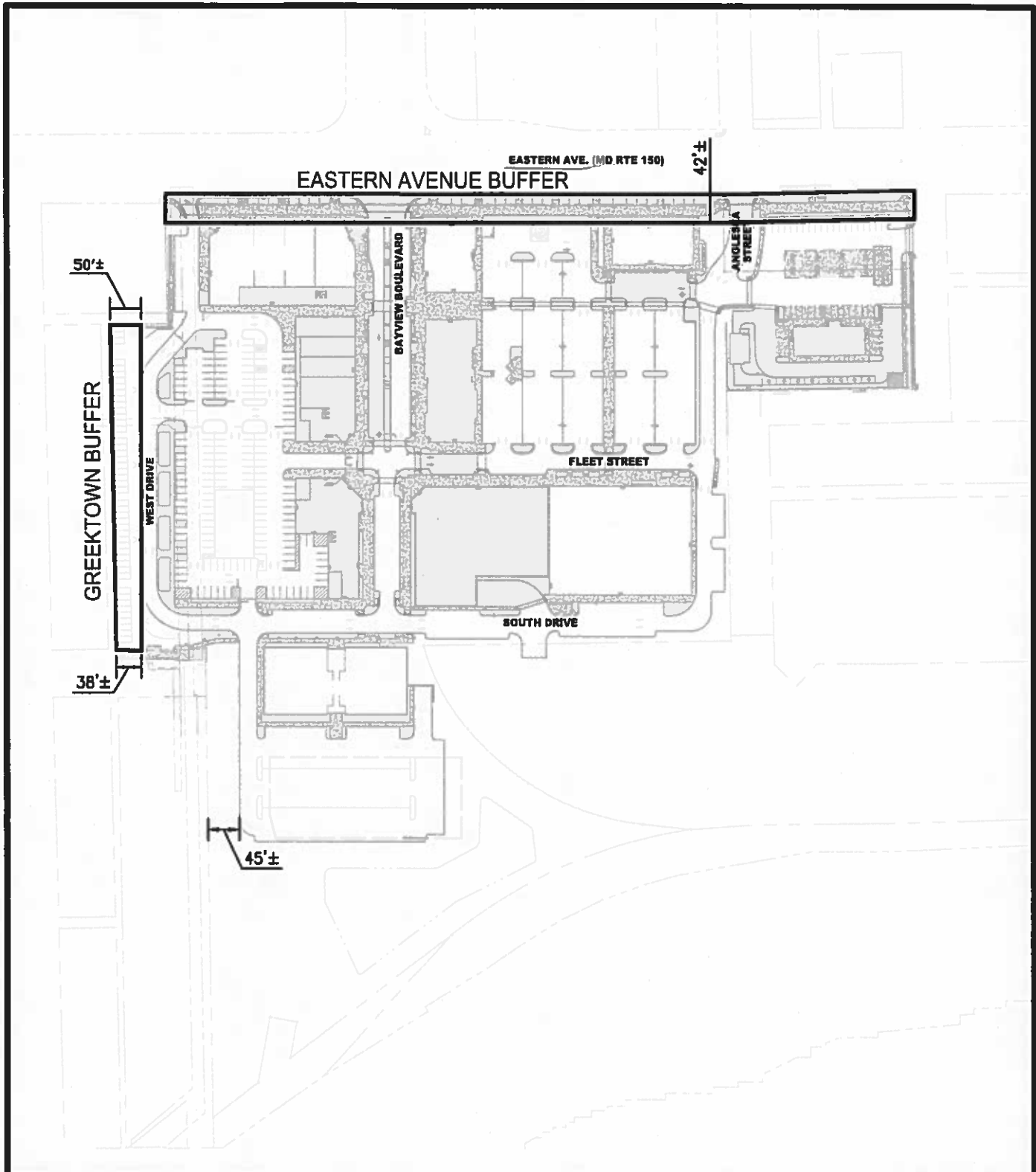
DATE: 07-12-2018

DRAWN BY:

DESIGN BY:

REVIEW BY: JTS

JOB NO. : 18297



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5601 EASTERN AVENUE

**DESIGN GUIDELINES
 EXHIBIT 'B'**

BUFFER AREAS

BALTIMORE CITY, MARYLAND

SCALE: 1"=200'

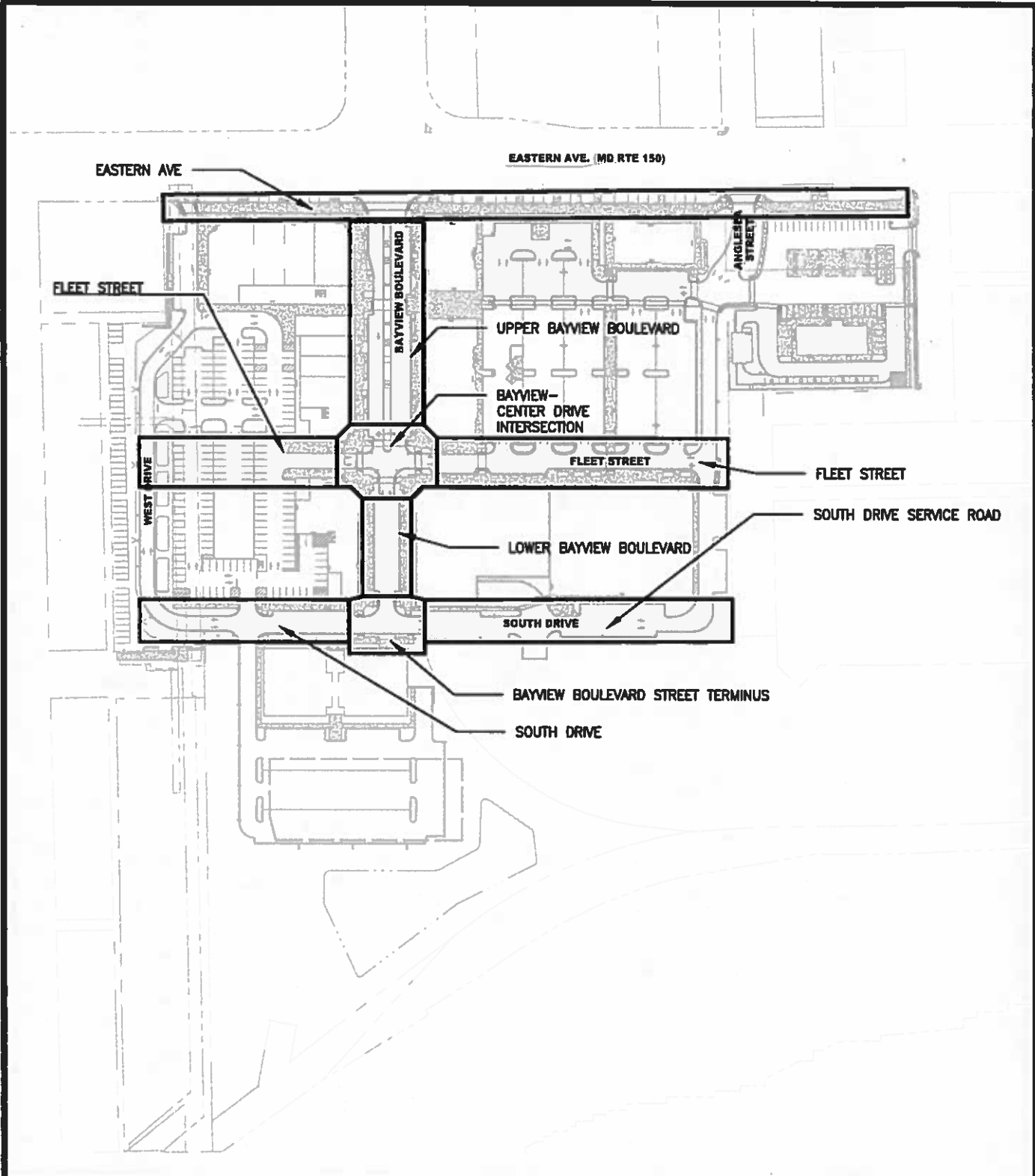
DATE: 07-12-2018

DRAWN BY:

DESIGN BY:

REVIEW BY: JTS

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5601 EASTERN AVENUE

**DESIGN GUIDELINES
 EXHIBIT 'C'**

STREETSCAPE AREAS

BALTIMORE CITY, MARYLAND

SCALE: 1"=200'

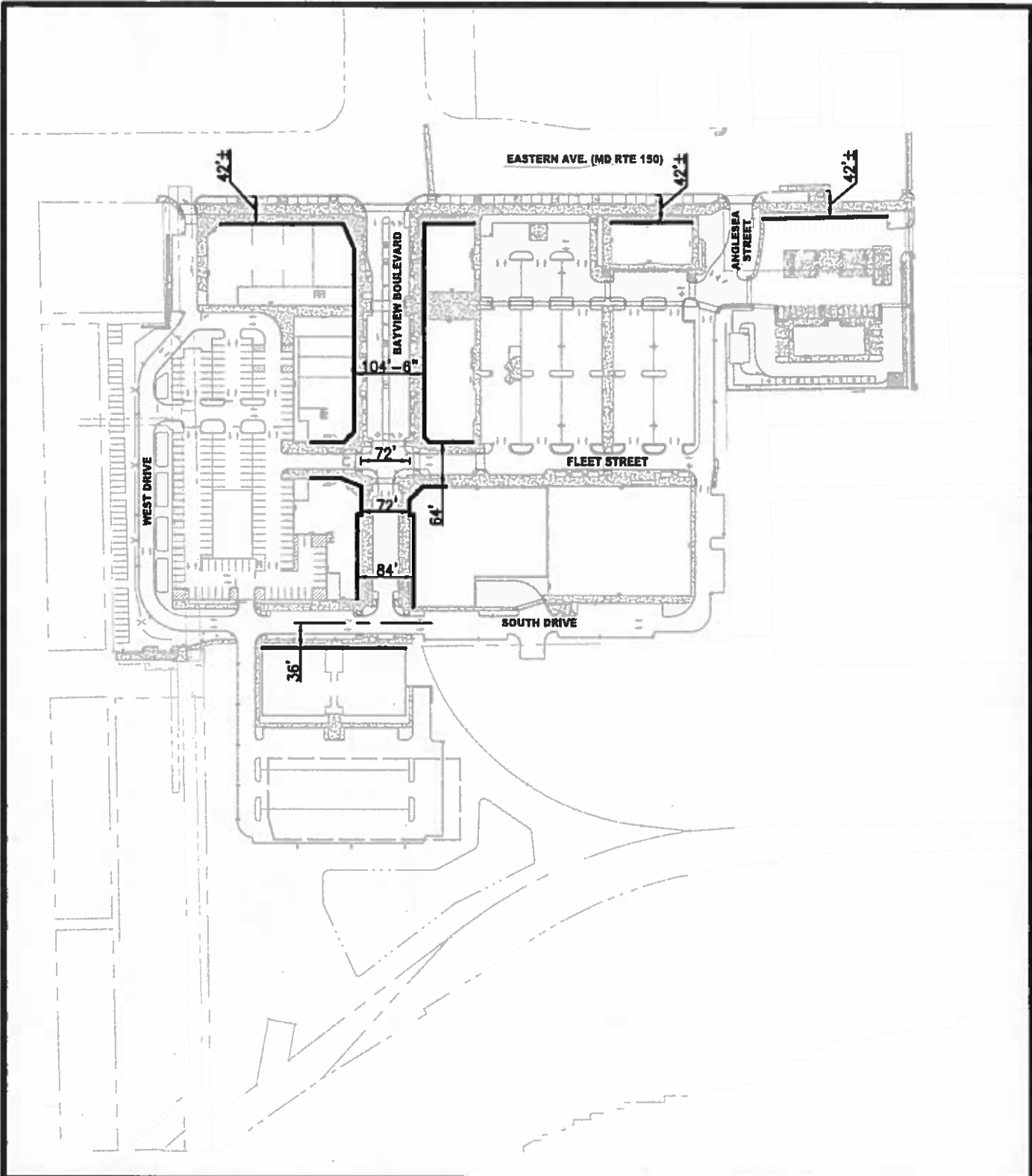
DATE: 07-12-2018

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5601 EASTERN AVENUE
DESIGN GUIDELINES
EXHIBIT 'D'

BUILD TO LINES
 DIMENSION IS AVERAGE OF PLANE

BALTIMORE CITY, MARYLAND

SCALE: 1"=200'

DATE: 07-12-2018

DRAWN BY:

DESIGN BY:

REVIEW BY: JTS

JOB NO. : 18297